

4.0 CULTURAL ENVIRONMENT

4.1 Cultural Heritage-Built Heritage and Cultural Landscapes

Two previous studies provided base line information regarding built heritage features and cultural landscapes within the analysis area. One of these was a 1981 study by Paul Dilse for a section of Highway 7 and 8 from east of Stratford to New Hamburg, entitled *Description of Man-Made Heritage in the Environment: Preliminary Report*. That study considered both standing structures and cultural heritage landscapes for the aforementioned portion of the current highway. Timmins Martelle Heritage Consultants Inc. also prepared a base line study of inventoried built heritage features within the entire analysis area. The latter study synthesized existing municipal and provincial inventories and provided a summary of the prior work of Dilse. It also noted that a thorough field review for the entire corridor had not been conducted and that many municipalities had either no official inventory of built heritage resources or only minimal ones.

The preferred corridor was considered with respect to buildings or standing sites of known or potential architectural significance, heritage bridges, areas of historic 19th century settlement and cemeteries. **Exhibit 4-1** maps the location of these heritage resources. **Exhibit 4-2** provides photos of the majority of these resources.

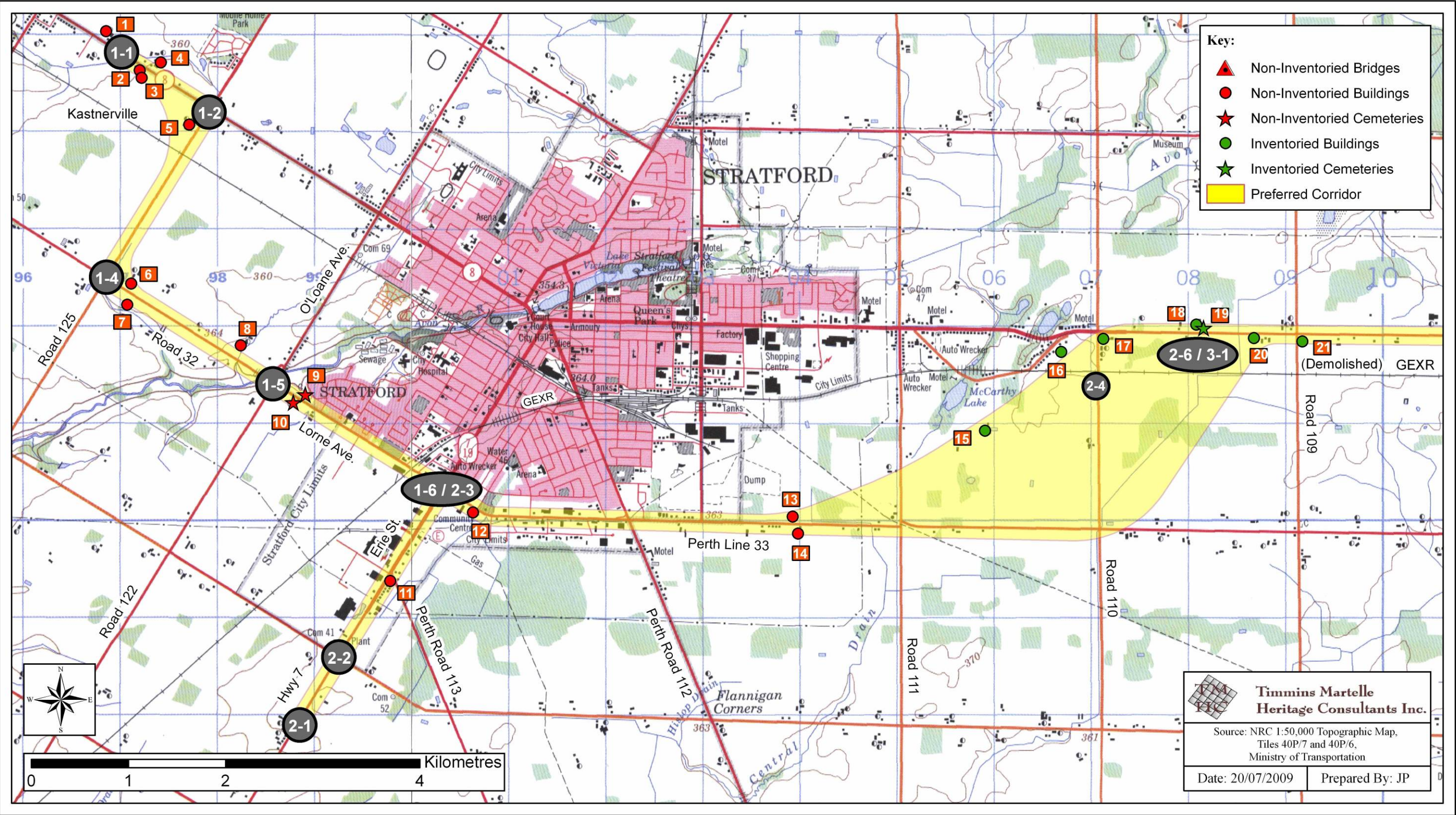


Exhibit 4.1 (a): Map of Built Heritage Resources Within the Preferred Corridor

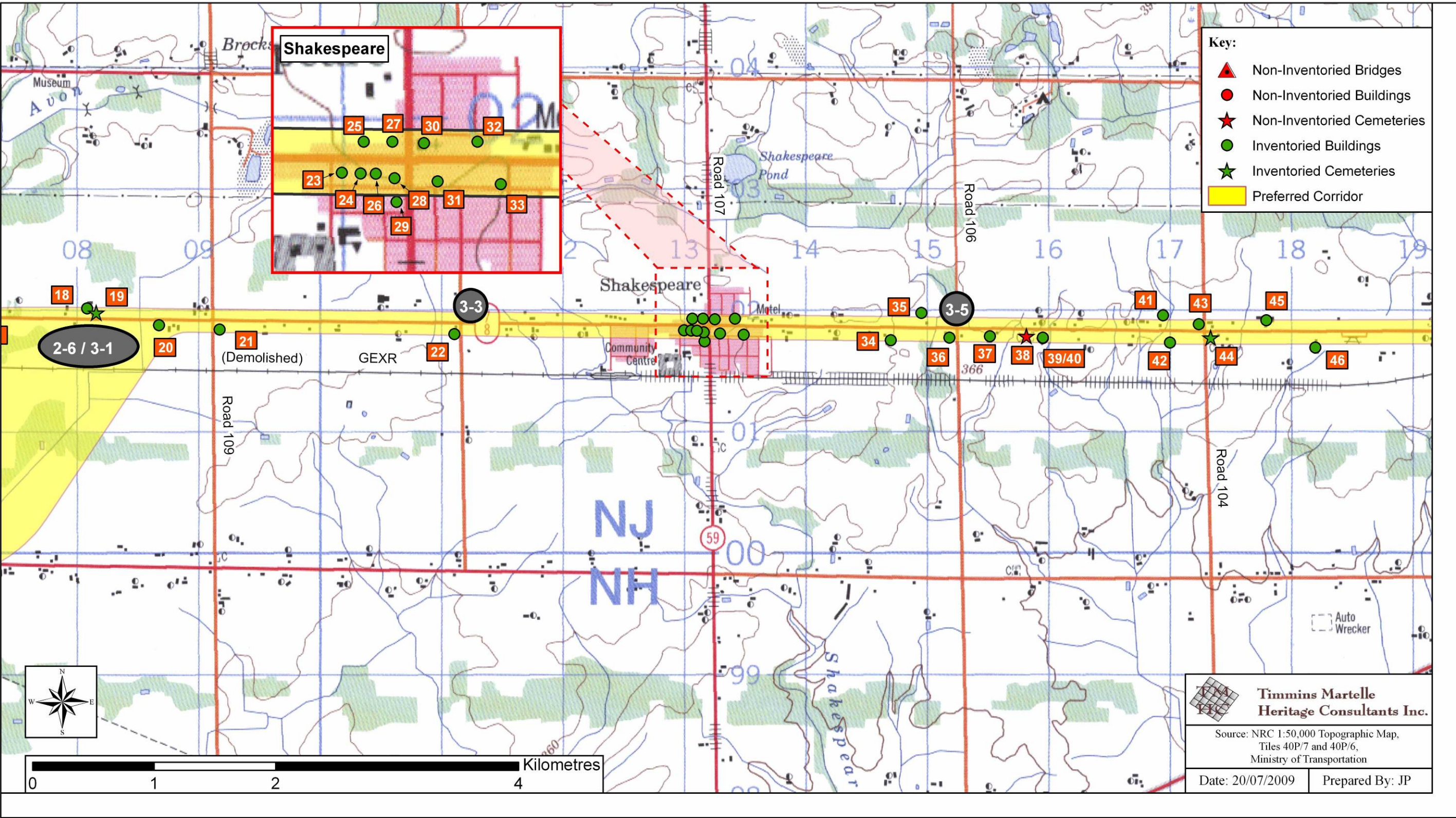


Exhibit 4.1 (b): Map of Built Heritage Resources Within the Preferred Corridor

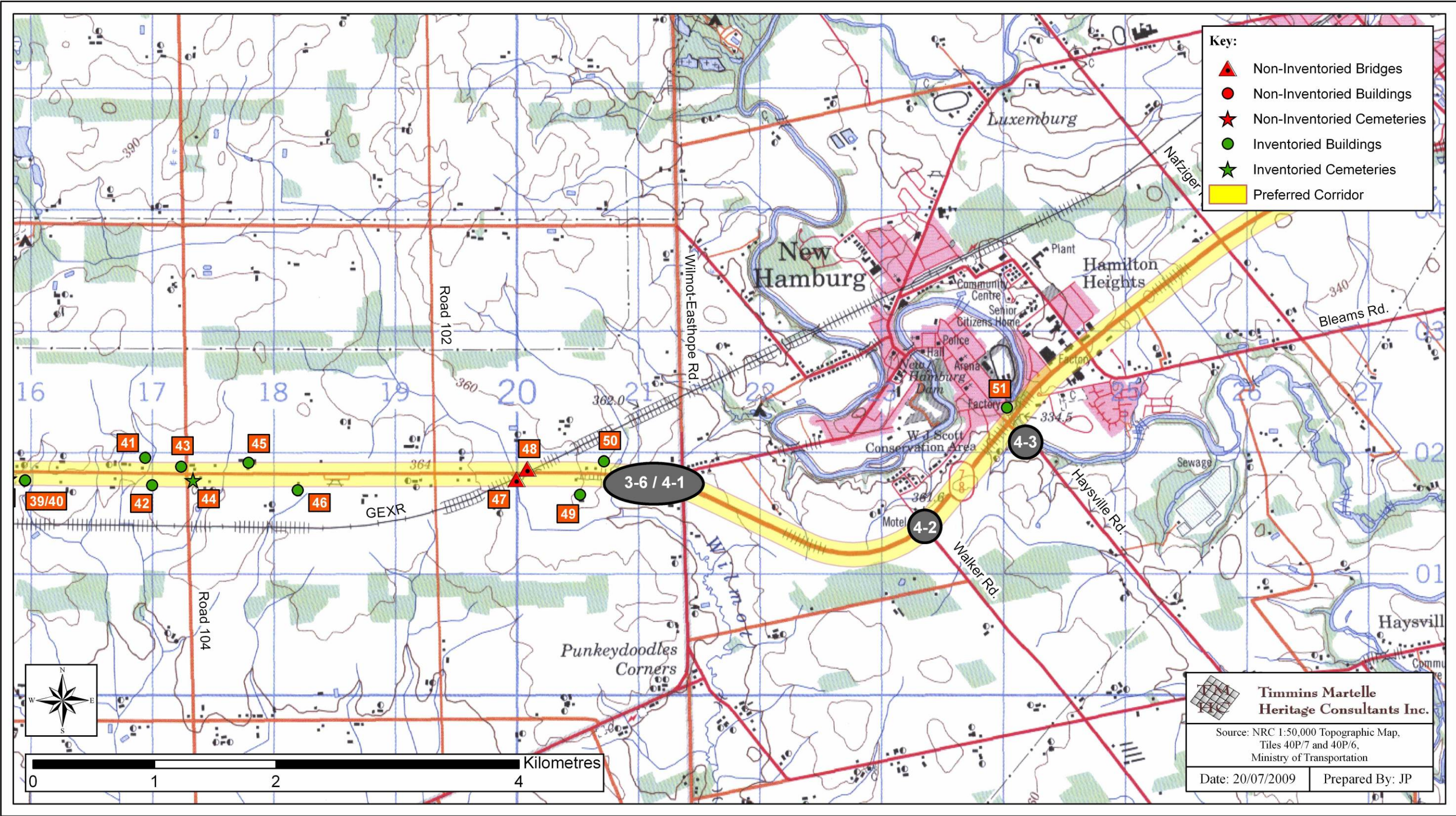


Exhibit 4.1 (c): Map of Built Heritage Resources Within the Preferred Corridor

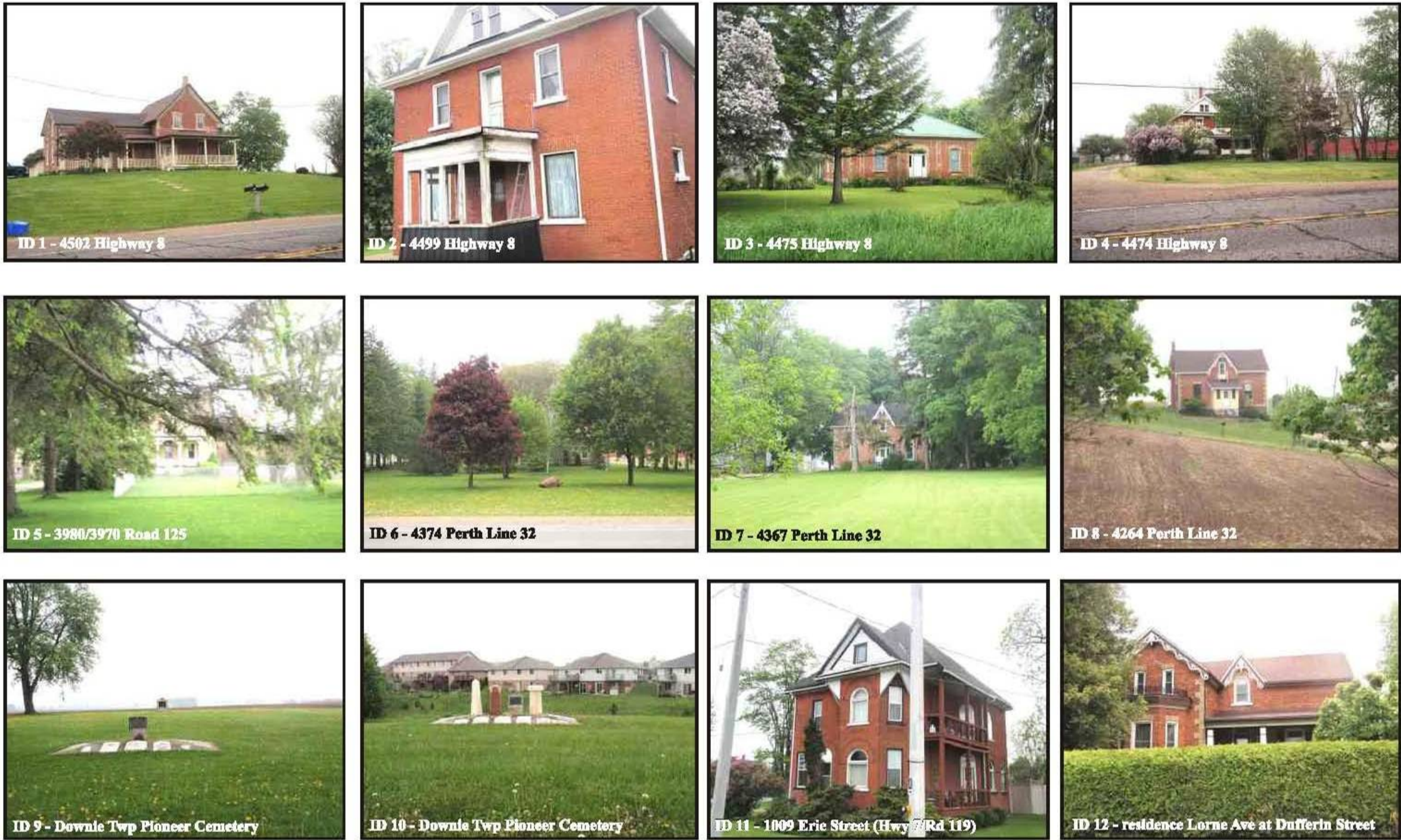


Exhibit 4-2(a): Photos of Heritage Structures within the Preferred Corridor

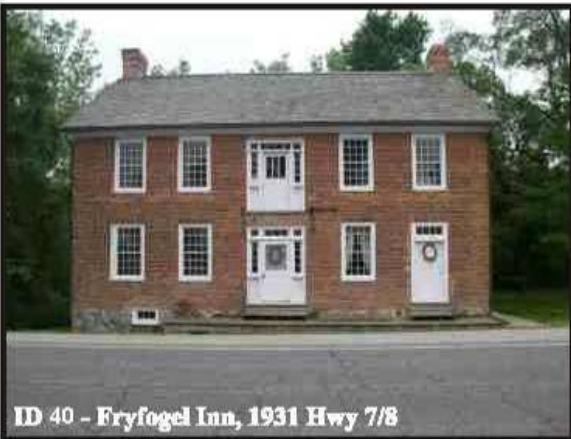


Exhibit 4-2(b): Photos of Heritage Structures within the Preferred Corridor

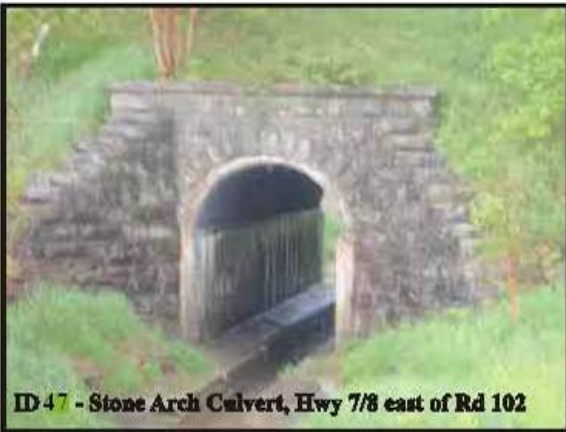


Exhibit 4-2(c): Photos of Heritage Structures within the Preferred Corridor

Exhibit 4-3 identifies each resource by name and address, and provides a brief description for some of them

Exhibit 4-3: List of Heritage Structures within the Preferred Corridor				
Name	I.D. Number On Map Exhibit 4-1	Status	Location/Address	Description
farmstead	1	extant	4502 Highway 8	(non-inventoried structure)
residence	2	extant	4499 Hwy 8	(non-inventoried structure)
residence	3	extant	4475 Hwy 8	
farmstead	4	extant	4474 Hwy 8	(non-inventoried structure)
farmstead	5	extant	3980/3970 Rd 125	(non-inventoried structure)
farmstead	6	extant	4374 Perth Line 32	(non-inventoried structure)
farmstead	7	extant	4367 Perth Line 32	(non-inventoried structure)
farmstead	8	extant	4264 Perth Line 32	(non-inventoried structure)
Downie Twp Pioneer Cemetery	9	extant	south side Lorne Avenue, east of Rd 122	(non-inventoried structure)
Downie Twp Pioneer Cemetery	10	extant	north side of Lorne Avenue, east of Rd 122	(non-inventoried structure)
corner residence	11	extant	1009 Hwy 7/Erie St./ Rd 119	(non-inventoried structure)
Klumper residence	12	extant	south side Lorne Avenue, east of Hwy 7	(non-inventoried structure)
farmstead	13	extant	3080 Perth Line 33	(non-inventoried structure)
farmstead	14	extant	3075 Perth Line 33	(non-inventoried structure)
James Reaney's Birthplace	15	extant	2919, 2921 Forest Road off Hwy 7/8	yellow brick Gothic Revival house (inventoried structure)
Barn and Shed	16	extant	2829 Hwy 7/8	Pennsylvania German barn and shed (inventoried structure)
McCallum Farmstead	17	extant	2797 Hwy 7/8	Gothic Revival house (inventoried structure)
James Rankin Farmstead	18	extant	2698 Hwy 7/8	Georgian house; Pennsylvania (inventoried structure)German barn
James Rankin Cemetery	19	extant	2698 Hwy 7/8 north side	
Alexander Anderson Farmstead	20	extant	3993 Road 109	Gothic Revival house; Central Ontario barn (inventoried structure)
Barn	21	demolished	Hwy 7/8 west of Road 109	Central Ontario barn (inventoried structure)
Barn	22	extant	Hwy 7/8 at Road 108	Pennsylvania German barn (inventoried structure)
Row of Gothic Revival Houses	23	extant	2215, 2217, 2219 Hwy 7/8 south side; 3997A & B Galt Street	Row of Gothic Revival Houses
House	24	extant	2213 Hwy 7/8 south side	
Commercial Block	25	extant	2204A 2204B Hwy 7/8 north side	
Cottage	26	extant	2209 Hwy 7/8 south	
Union Hotel	27	extant	Hwy 7/8 north side	
The Capeling House	28	extant	Hwy 59 near Hwy 7 & 8 in Shakespeare	
Hall	29	extant	2202 Hwy 59 (Perth Road 109) west side	

Exhibit 4-3: List of Heritage Structures within the Preferred Corridor				
Name	I.D. Number On Map Exhibit 4-1	Status	Location/Address	Description
Shakespeare Presbyterian Church	30	extant	2196 Hwy 7/8 north side	
Gothic Revival House	31	extant	Hwy 7/8 near Hwy 59 (Perth Road 107 / Woodstock St)	
Georgian House	32	extant	Hwy 7/8 in Shakespeare; north side	
Small House	33	extant	Hwy 7/8 in Shakespeare; south side	
Andrew Riddell Junior Farmstead	34	extant	2053 Hwy 7/8	Gothic Revival house; rubblestone outbuilding and barn (inventoried structure)
John McTavish Farmstead	35	extant	2026 Hwy 7/8	rubblestone Georgian House; outbuilding (inventoried structure)
Andrew Riddell Farmstead	36	extant	2007 Hwy 7/8 at Road 106	Pennsylvania German barn; house (inventoried structure)
Dr. Flynn's House	37	extant	1971 Hwy 7/8	house of first doctor; red brick painted cream (inventoried structure)
Fryfogel Cemetery	38	extant	Hwy 7/8 near Fryfogel Inn	
Sebastian Fryfogel Farmstead	39	extant	1899 Hwy 7/8	Georgian stucco house; Pennsylvania German barn (inventoried structure)
Fryfogel Tavern	40	extant	1931 Hwy 7/8	1844; historic inn; Georgian (inventoried structure)
Outbuilding	41	extant	1834 Hwy 7/8	Rubblestone (inventoried structure)
George Kleinknecht Farmstead	42	extant	1825 Hwy 7/8	mid 19th c shed; Pennsylvania Dutch barn (inventoried structure)
Lingelbach Evangelical Church	43	extant	1802 Hwy 7/8	yellow brick Gothic Revival (inventoried structure)
Lingelbach Cemetery	44	extant	3995 Road 104	
First Koch Farm Barn	45	extant	1748 Hwy 7/8	Pennsylvania German barn (inventoried structure)
Charles Koch Farmstead	46	extant	1709 Hwy 7/8	shed and Central Ontario barn (inventoried structure)
Stone Arch Culvert	47	extant	Hwy 7/8 at railway crossing	
Railway Overpass	48	extant	Hwy 7/8 at railway crossing	
Otto Family Farmstead	49	extant	1481 Hwy 7/8	Georgian house, Pennsylvania German barn (inventoried structure)
Shildroth Family Farmstead	50	extant	1466 Hwy 7/8	yellow brick Italianate house 1880 (inventoried structure)
Frank-Rau Brewery	51	extant	29 Bleams Road East	1850s brewery converted to cheese factory (inventoried structure)

4.1.1 Buildings or Standing Sites of Architectural Significance

Twenty-three previously inventoried heritage buildings fall within the preferred corridor and its buffer. All of these were inventoried during Dilse's 1981 study and fall within his study area between Stratford and New Hamburg. The majority of buildings are 19th century structures (residences, barns, former taverns, outbuildings, factories) fronting Highway 7&8 or in close proximity to it. The majority of these buildings do not appear on official municipal inventories. However, one building – the Fryfogel Inn - appears on the inventory of the Township of Perth East and is listed in the *Ontario Heritage Properties Database*. The Fryfogel Inn, as well as its associated homestead site and cemetery, is of great significance to the local community. An Ontario Heritage Foundation (now Ontario Heritage Trust) plaque marks the property. **Exhibit 4-3** indicates which of the heritage buildings are inventoried structures, but they include James Reaney's Birthplace near Little Lakes, the McCallum, Rankin, Anderson, Riddell, McTavish, Kleinknecht, Koch, Otto and Shildroth farmsteads, the Frank-Rau Brewery and the Lingelbach Evangelical Church. Three buildings on Dilse's inventory have since been demolished.

Because complete municipal heritage inventories do not exist for some sections of the preferred corridor, a preliminary field review was also conducted to identify non-inventoried buildings of potential heritage significance. **Exhibit 4-3** indicates which of the heritage buildings are non-inventories structures, but they are restricted to the portions of the corridor west and south of Stratford. There is a concentration of non-inventoried heritage structures (residences and farmsteads) on Highway 8, surrounding the historic settlement community of Kastnerville. There is another residence and farmstead just south of Highway 8 on the west side of Perth Road 125. Two additional farmsteads occur on the north and south sides of Perth Road 32, just west of its intersection with Perth Road 125. Another 19th century farmstead appears on the north side of Perth Road 32 just west of a small tributary that drains into the Avon River. There is a two-storey red brick structure occupying a south corner lot of the intersection of Highway 7 (Erie Street – Road 119) and Embro Road (Road 113), within an historical settlement area. There is one non-inventoried heritage structure on the south side of Lorne Avenue east of Highway 7 and now within a relatively built up residential area. Two additional non-inventoried farmsteads appear on the north and south sides of Perth Road 33, southeast of Stratford near the municipal landfill site.

4.1.2 Heritage Bridges

There are no inventoried heritage bridges within the preferred corridor and its buffer. However, municipal inventories that exist are incomplete and few pay particular attention to bridges. The only currently identified potential heritage resources of this nature are a railway overpass on Highway 7&8 east of Perth Road 102 and an associated stone arch culvert at the creek crossing in the same location. Both are identified in **Exhibit 4-1** and **Exhibit 4-3**. The Lorne Avenue Bridge at Road 122 (O'Loane Avenue) was constructed in 1969 and subsequently is not considered in this study as it does not qualify as a heritage structure based on provincial guidelines.

4.1.3 Areas of 19th Century Settlement

There are several areas of early and important 19th century settlement within the preferred corridor and its buffer. Highway 7&8 once formed part of one of the earliest roads constructed by the Canada Company – first named the Goderich Road and later the Huron Road. It was opened in 1828 and connected two major planned centres established by the Canada Company – Goderich, on the shore of Lake Huron, and Guelph. Many early- to mid-19th century communities and homesteads can be found along the highway proper. The homesteads are represented by both inventoried and non-inventoried heritage structures. Significant communities along the corridor include Kastnerville (east of Sebringville), the Little Lakes area, Shakespeare and New Hamburg. The historic town core of New Hamburg will not be affected. There is a small historic cross-roads centre at the intersection of Highway 7 (Road 119) and Embro Road (Road 113), south of Stratford.

4.1.4 Cultural Heritage Landscapes

A comprehensive study of cultural heritage landscapes within the preferred corridor and its buffer zone has not yet been conducted. However, Disle's 1981 study identified landmarks and views within his then defined study area, some of which fall within the preferred corridor. These are listed as:

1. the view of New Hamburg and the Baden Hills from Highway 7 & 8 between Bleams Road and the Waterloo-Perth boundary;
2. the Shildroth Farmstead and neighbouring Otto Farmstead, both identified in **Exhibit 4-1** and **Exhibit 4-3**; and
3. the landscape along Highway 7 & 8 from Lingelbach Cemetery (Road 104); to the Andrew Riddell Junior Farmstead west of Sideroad 15 (now Road 106) and just east of Shakespeare (both locations are also identified in **Exhibit 4-1** and **Exhibit 4-3**).

Dilse also emphasized the significance of the scenic landscape along Highway 7&8 in general, between the Waterloo-Perth boundary and Stratford's eastern limits, noting its historical nature. He noted that "the landscape seen from this stretch of highway is the evolution of the landscape seen from the Huron Road, the colonization road of the Canada Company's large tract" (1981:17).

4.1.5 First Nations Burial Sites

There are no known First Nations burial sites within the preferred corridor or its buffer.

4.1.6 Cemeteries

There are six known cemeteries within the preferred corridor and its buffer. These all have roots in the 19th century. Three of these were inventoried by Dilse in 1981 and the remaining three do not appear on heritage registers. The Fryfogel Cemetery is part of the larger cultural complex including the Fryfogel Tavern and Fryfogel Homestead. There are two small pioneer cemeteries on Lorne Avenue east of Road 122. One of these is on the north side of the road and the other

on the south. The James Rankin Cemetery is a family plot associated with the James Rankin Homestead on Highway 7&8 east of Little Lakes. The South Easthope Cemetery is on the east side of Perth Road 107 south of Shakespeare, and will not be affected by the preferred corridor. The Lingelbach Cemetery rests in the southeast corner of the intersection of Highway 7&8 and Road 104. These cemeteries are identified in **Exhibit 4-1** and **Exhibit 4-3**.

4.2 Cultural Heritage – Archaeology

The preferred corridor was considered in terms of potential for impacts to known and previously undocumented archaeological resources.

4.2.1 Registered Archaeological Sites

Registered sites within the preferred corridor and its associated buffer are listed in **Exhibit 4-4**. The majority of these were identified during archaeological assessments conducted for previous improvements to the Highway 7&8 corridor. They all occur adjacent to the existing Highway 7&8 corridor, east of Stratford. Some sites have been partially or fully mitigated and some are still completely intact. Those that remain a planning concern for this project are shown in the table below. These are predominantly EuroCanadian industrial and homestead sites. Most are associated with extant heritage buildings. The currently proposed highway improvements have the potential to impact these sites, if construction plans incorporate any portion of the site areas. If impacts to these areas are identified, mitigation through excavation or avoidance will be necessary.

Exhibit 4-4			
Registered Archaeological Sites within the Preferred Corridor			
Borden	Name	Type	Age
AiHf-1	Easthope Kiln	industrial	EuroCanadian
AiHf-2	Heinkel	unknown	unknown
AiHf-3	Stratford-Little Lakes	unknown	unknown
AiHf-4	Stratford-Little Lakes	homestead	EuroCanadian
AiHe-14	Bergey 1 & 2	Archaic	camp
AiHe-20	Boehler Pottery	industrial	EuroCanadian
AiHe-21	Fryfogel Inn	tavern	EuroCanadian
AiHe-22	Riddell 1	homestead	EuroCanadian
AiHe-23	Riddell 2	homestead	EuroCanadian
AiHe-24	Fryfogel	homestead	EuroCanadian
AiHe-25	Amacher	homestead	EuroCanadian
AiHe-26	Guilk	homestead	EuroCanadian
AiHe-27	Buschler	homestead	EuroCanadian

4.2.2 Potential for Undocumented Archaeological Sites

The potential for lands to contain previously undocumented archaeological sites is determined through the consideration of provincially-established criteria. According to the Ministry of Culture's *Checklist for Determining Archaeological Potential*, any lands falling within:

- 250 metres of a known archaeological site
- 300 metres of a potable water source (extant or ancient)
- an area of elevated topography (knolls, drumlins, eskers, plateaus)
- pockets of sandy soil in a clay or rocky area
- an extractive area (for food or scarce resources)
- an area of early European settlement
- proximity to an historic transportation route
- an *Ontario Heritage Act* designated property
- or containing unusual land formations (mounds, caverns, waterfalls)

have potential for the discovery of archaeological resources. Proximity to water is the primary factor determining archaeological site locations. Several primary, secondary and tertiary watercourses cross the preferred corridor. The northwest section of the corridor falls within the Thames River Drainage System and is drained by Black Creek, the Avon River, Trout Creek and related tributaries. The central and east portion of the corridor falls within the Grand River Drainage System and is drained by the Nith River, Silver Creek, Wilmot Creek, Horner Creek and Baden Creek.

New construction along the preferred corridor and within its buffer has the potential to impact previously undocumented archaeological resources. Areas within the corridor and buffer meet Ministry of Culture criteria for lands having archaeological potential. Based on the above criteria, archaeological potential was mapped and plotted in **Exhibit 4-5**.

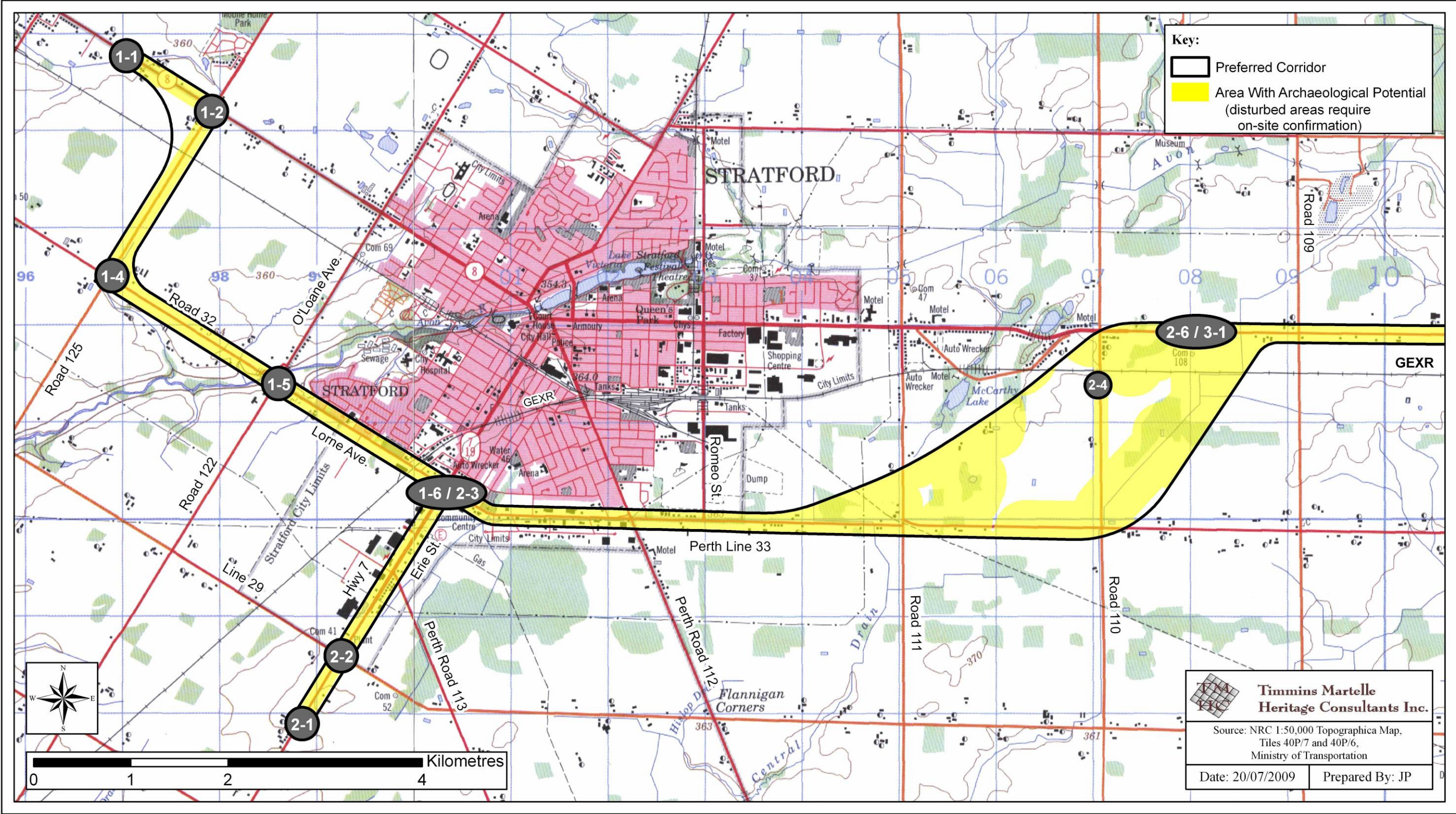


Exhibit 4.5 (a): Map of Archaeological Potential within the Preferred Corridor

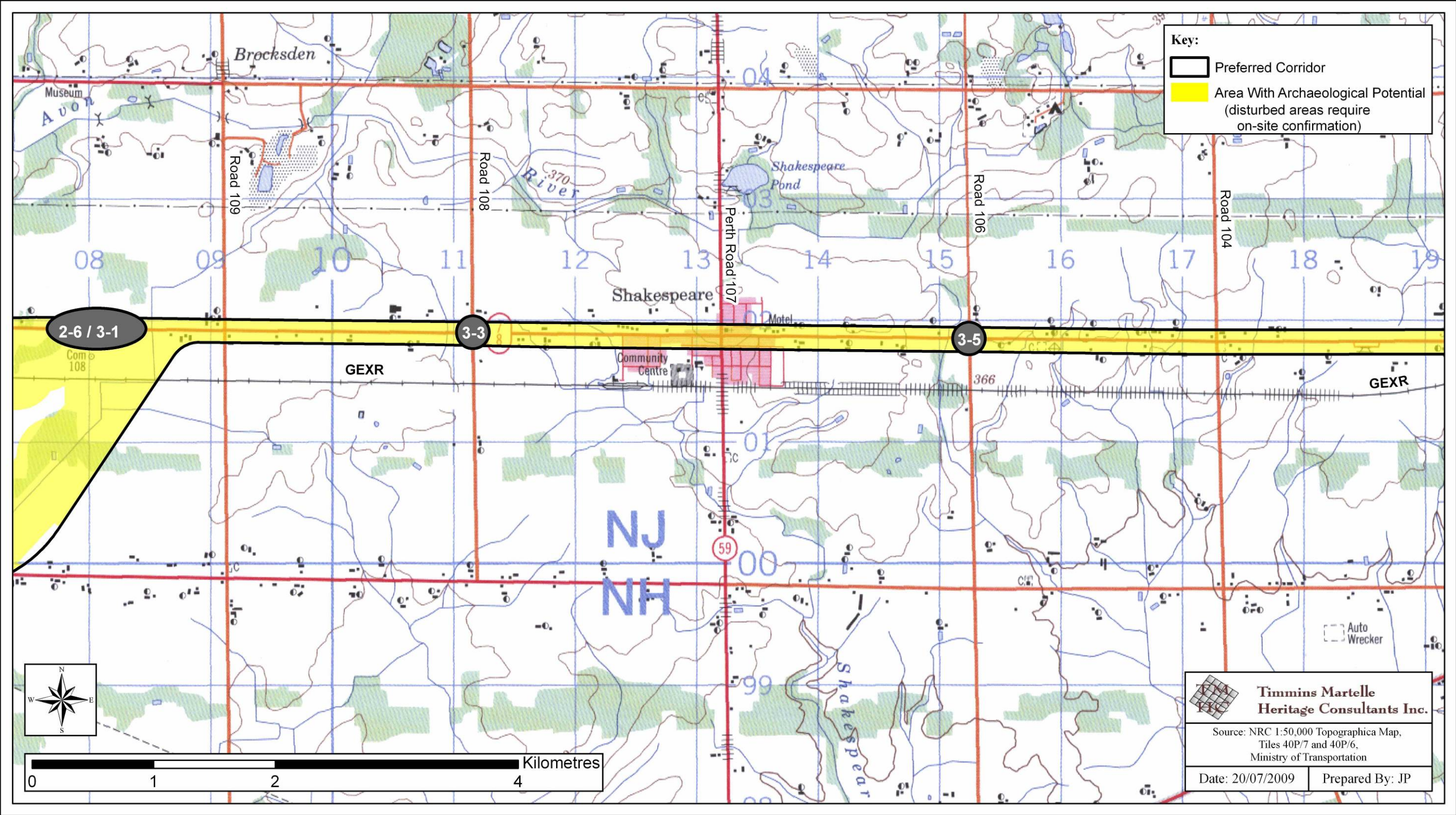


Exhibit 4.5 (b): Map of Archaeological Potential within the Preferred Corridor

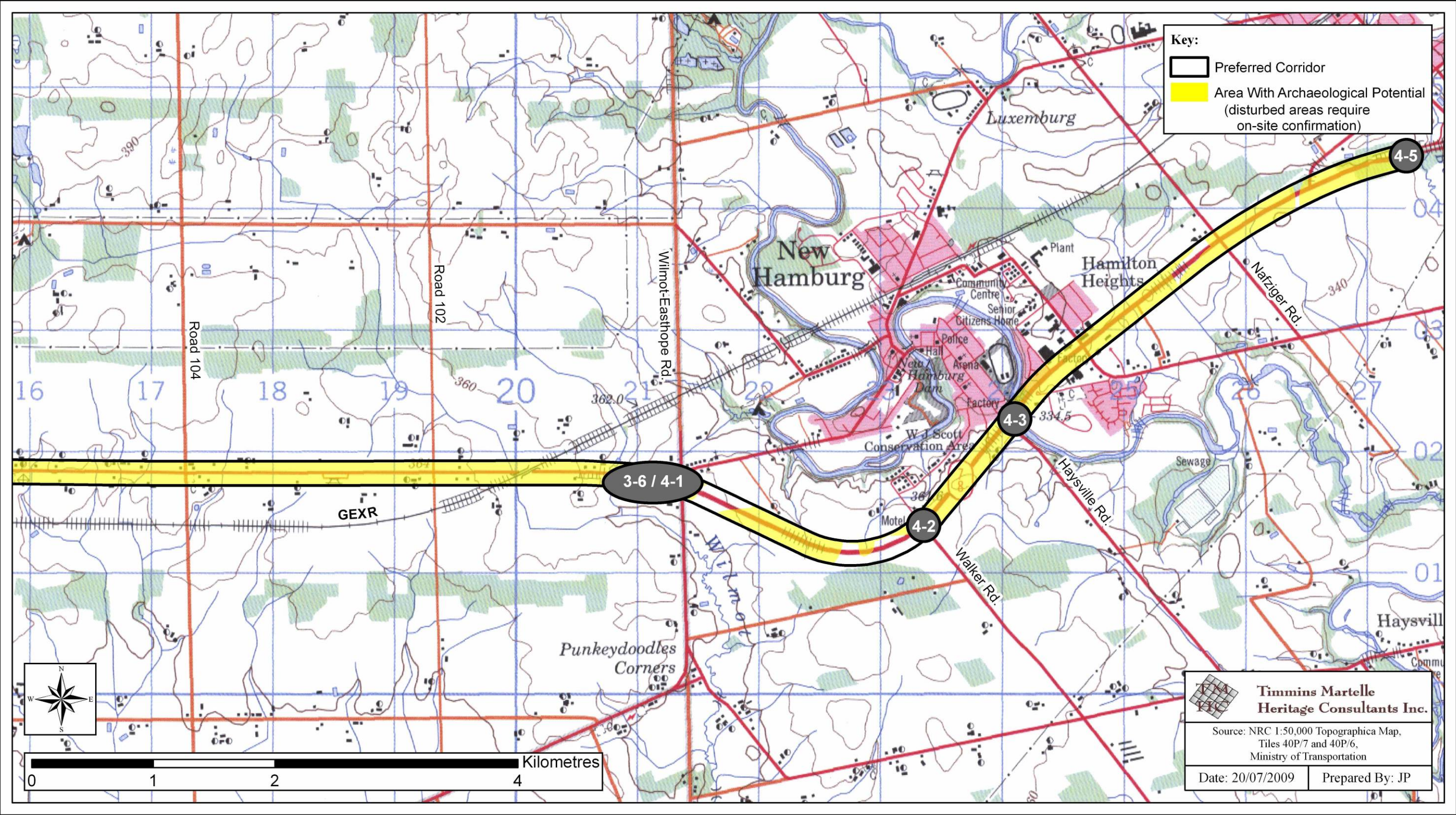


Exhibit 4.5 (c): Map of Archaeological Potential within the Preferred Corridor

4.2.2.1 Pre-Historic and Historic First Nations Archaeological Sites

Potential for First Nations archaeological sites is determined primarily on the physiographic features described above (proximity to water, elevated topography, well-drained soil). Given this, many areas within the preferred corridor have potential for the discovery of as yet undocumented First Nations archaeological sites.

4.2.2.2 Historic Euro-Canadian Archaeological Sites

Potential for historic Euro-Canadian archaeological sites is determined primarily based on proximity to water, elevated topography, historic transportation routes, extractive areas and locales of early Euro-Canadian settlement. Historic era archaeological sites are also found in association with extant 19th century buildings. Report F (Part 1) described the importance of Highway 7&8 as an early colonization road created by the Canada Company in 1828. It also noted that most of the major concession roads in the affected portions of Perth County and Waterloo County were open by the last quarter of the 19th century. Several early centres of settlement were noted including Stratford, Shakespeare and New Hamburg, in addition to the entire historic Highway 7&8 corridor.

Based on this information, many lands within the preferred corridor and its buffer have high potential for the discovery of historic Euro-Canadian archaeological resources. Areas most affected in this way are those peripheral to the existing highway and concession roads (routes that were open in the 19th century), as well as extant heritage structures.

4.2.3 Project Area and Application of Specific Information

Another miscellaneous criteria is also considered when evaluating archaeological potential. This is extent of recent disturbance and subsequent probability for the recovery of intact archaeological deposits. The disturbance of natural soil horizons through major development-related activities, primarily road construction, subdivision development, and infrastructure projects, can negate potential for the recovery of archaeological resources. Throughout the preferred corridor there are several locales where prior, extensive disturbance can be confirmed, including along roadways and in the more urban zones. Other, less noticeable, disturbance likely also exists in several areas, but this would have to be confirmed by an on-site inspection.